

# REPORT

## City of Swift Current

### Infill Guidelines



**As Amended by City Staff  
December 2014**



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## 1 Introduction

Infill development involves new development that is sited on vacant or underdeveloped land within an existing community, and is generally surrounded by existing development. Infill developments do not include renovations or other rehabilitative works to existing housing stock. It is commonly referred to as “filling the gaps.” In residential areas it provides the opportunity to create additional housing stock by developing the vacant land, provides an opportunity for a variety of residential development to take place, and provides for the rejuvenation of the area.

Infill development can benefit existing communities in a variety of ways. It helps to revitalize and renew old housing stock in existing mature areas which helps to increase the real estate and property values of the existing mature area. Existing infrastructure is better utilized through infill development and public service costs such as sidewalks, water and sewer, and public safety (fire, hospital, and police) tend to be lowered. It also can assist a community in achieving thresholds of population density necessary for amenities involving park space and community services.

The City’s Planning and Growth Department, as part of its Housing Plan, identified the need to provide guidance for future residential infill development in the City of Swift Current’s mature areas. Residential Infill Guidelines are an important tool in fostering infill development and achieving the City of Swift Current’s Housing Plan’s goals and objectives. These residential infill guidelines are a companion document to the City of Swift Current’s Zoning Bylaw and are intended to be an appendix to the Official Development Plan as policy guidelines and performance expectations for infill projects, concept plans, and development proposals.

## 2 Objectives

### 2.1 CITY OBJECTIVES

1. To contribute to the creation of livable mature areas through residential infill by:
  - a. Developing animated and secure streets and open spaces;
  - b. Promoting high quality development, building materials, and design; and
  - c. Providing for the inclusion of onsite and offsite amenities.
2. To strive to secure community support and acceptance for residential intensification in mature areas:
  - a. By minimizing traffic and parking impacts that may result from intensification;
  - b. By providing for a more equal distribution of density among areas of the City; and,
  - c. By ensuring that infill is compatible in terms of scale and architecture with the existing and adjacent development.
3. To make more efficient use of existing infrastructure and community facilities.
4. To contribute to the physical renewal of older neighbourhoods through:
  - a. New housing;

- b. New and/or upgraded infrastructure, including storm and sanitary sewer, roads, lanes and sidewalks, boulevard landscaping and lighting;
  - c. Affordable housing; and,
  - d. Additional amenities such as green space, recreational facilities, and landscaping.
5. To achieve housing forms that contribute in the long term to:
- a. Increased housing choice in mature areas (for example, seniors, families, and student housing); and
  - b. An increased supply of more affordable housing and non-market housing;
6. To reduce delays in the review of applications for residential infill due to conflict and a lack of clear policy direction.

## 2.2 NEIGHBOURHOOD OBJECTIVES

1. To maintain a balanced mix of housing types within each existing area and to distribute density amongst mature areas.
2. To protect the stability of single family areas.
3. To develop animated and secure public streets and open spaces.
4. To minimize vehicular traffic and parking impacts that may result from intensification.
5. To secure improvements to existing mature areas infrastructure as part of redevelopment.
6. To provide additional housing opportunities that:
  - a. Allow seniors to “age in place” in their communities;
  - b. Make more homes available for families; and
  - c. Provide more affordable housing.
7. To encourage the retention of existing, housing stock which is in good condition and offers affordable family housing options.
8. To support the replacement of deteriorated housing stock with an equivalent amount of housing that is equally affordable.
9. To reduce the amount of time and effort required of the community to oversee development activity in the affected area as the guidelines will provide policy direction in the development of proposed projects.

## 2.3 DEVELOPMENT OBJECTIVES

1. To provide clarity to potential developers concerning the City’s desired form of infill development and the municipal application and approval process.
2. To recognize the developer’s role in addressing the economics of housing affordability and choices across the continuum of the housing market.
3. To provide flexibility in the regulation of residential infill projects to deal with the context within which projects occur by developing infill guidelines that:
  - a. Can be applied universally but will cover a range of situations; and
  - b. Are performance based guidelines rather than detailed regulations.



4. To recognize that the existing mature areas of the City are already located within a zoning district. The existing zoning districts will be inherent to the infill development which provides the opportunity to develop more intensive and inclusive housing options on underutilized lands.
5. To ensure that developers provide an appropriate mix of alternative forms of housing that reflect the market demand and support the diversification of the City's housing stock.
6. To encourage developers to exceed the minimum standards for open space dedications and local recreational improvements.

## 3 General Guidelines

### 3.1 SOCIALLY SUSTAINABLE

Residential infill projects should contribute to the creation and maintenance of sustainable mature areas providing traditional character, pedestrian friendly design, ensuring privacy and sunlight penetration on adjacent properties. Infill projects should contribute to improved social sustainability of the City's mature areas through such initiatives as:

1. Consulting with the community through the planning and design process;
2. Providing for a range of housing types across the continuum that includes housing for seniors, families with children, and affordable housing; and
3. Maintenance and improvement of community recreation facilities and amenities.

### 3.2 ECONOMIC AND FISCAL SUSTAINABILITY

Residential infill projects should contribute to the creation and maintenance of a more economically and fiscally sustainable City. Residential infill projects in mature areas should contribute to the economic and fiscal sustainability of the City by:

1. Using existing water, sewer, drainage and transportation infrastructure more efficiently and supporting the upgrading of this infrastructure where required;
2. Developing residential densities and area forms that will support efficient use of community services; and
3. Maintaining viable areas in which property owners maintain their dwellings and population levels support the existing areas services.

### 3.3 SINGLE FAMILY HOUSING

A critical mass of single family housing should be protected in the core of mature areas of the City. This will be accomplished by:

1. Allowing a modest amount of small scale infill (1-3 lots) within the interior of existing mature areas of the City; and

2. Directing the majority of infill to the edges of mature areas or onto large sites.

It is an objective in the City's Housing Plan to retain a balanced blend of housing within mature areas of the City. This contributes to the objective of maintaining stable, livable family-oriented areas which provide for a variety of housing types and options. Focusing infill development on the vacant lands of existing mature areas provides the opportunity to create new blocks which help to minimize parking and traffic impacts on the interior of the mature area. These vacant lands also provide the opportunity to develop concept plans which address the existing mature areas character, roadway patterns, development densities, and land uses which will be proposed.

### **3.4 MEDIUM DENSITY DEVELOPMENT**

Medium density infill development which encompasses a mixture of single-detached and various forms of multi-unit housing increases the efficiency of land use by two to five times over that associated with exclusively single-detached developments. Medium density infill development should be focused on the edge of existing mature areas of the City.

Directing medium density infill to the interior of larger scale sites that are adjacent to low density single detached housing will:

1. Encourage the revitalization of those areas that are generally in the greatest need; and
2. Create opportunities for sustainable community focal points to be shared by existing mature areas of the City.

### **3.5 AFFORDABLE AND SOCIAL HOUSING**

Affordable housing should be provided and consideration should be made to support the establishment of social housing in residential infill projects in accordance with current the City's Housing Plan through federal, provincial and municipal partnerships. An objective related to residential infill is to improve the supply of affordable housing. Large site infill developments will be required to include an affordable housing component as determined by the City's Housing Plan.

### **3.6 REAR LANE ACCESS**

Residential infill developments should respect the role of lanes not only as a secondary vehicular access route but as a factor in maintaining the livability of the mature areas. Importance should be placed on the public land of lanes as part of both a service and pedestrian network. If lanes are closed they should be retained for pedestrian movement through a linear park. Lanes should be considered, only if the adjacent existing mature area has lanes, in the design of infill development and kept attractive through fencing and landscaping, and appropriate design of parking areas and garages.

### **3.7 LOCATION TO ROADWAYS**

Low density residential infill is encouraged on the vacant lands of existing mature areas in proximity to local roadways. Local roadways are smaller in width and would provide suitable access for single family dwellings to be developed because of the limited traffic volumes which would be generated. Proposed medium and higher density residential infill development is encouraged on sites in close proximity to main arterial roadways which are wider than local roadways and can accommodate larger volumes of traffic.

### **3.8 PEDESTRIAN MOVEMENT**

Infill development should foster a high quality public land, a comfortable environment for walking, and new or improved connections within an existing mature area. Sidewalks should be provided that tie into existing pedestrian networks. They should be sufficiently wide and well maintained to encourage walkability and be constructed using a high standard of materials and treatments. Infill development should respect existing pedestrian movement patterns and should look for opportunities to provide new or improved connections. Medium and large scale developments should provide landscape plans that include public lands. Where appropriate, a high standard of planting, sidewalk and boulevard improvements, including street furniture, should be provided.

### **3.9 MASS AND SCALE OF EXISTING NEIGHBOURHOOD**

Infill development should respect the mass and scale of adjacent development and the character and attributes of the existing streetscape. Infill development should fit comfortably into existing mature areas of the City and reflect the character of the existing streetscape.

### **3.10 MECHANICAL SYSTEMS**

Mechanical systems should be located to ensure that noise does not impact adjacent residences. The location of mechanical systems such as air conditioning units and exhaust fans should ensure that noise from these systems does not impact on adjacent residences.

### **3.11 MEDIUM AND HIGH DENSITY CONSIDERATION**

Medium and high density residential development, encompassing a range of building forms from townhomes to apartment blocks, should be designed to accommodate an aging population. These types of developments should be sensitive to different segments of society including seniors and persons with physical disabilities. Special consideration should be given to features such as building and unit access, design of indoor and outdoor recreation and amenity space, lighting, signage, and safety features to accommodate different tenants.

## 4 Lot Size and Design Requirements

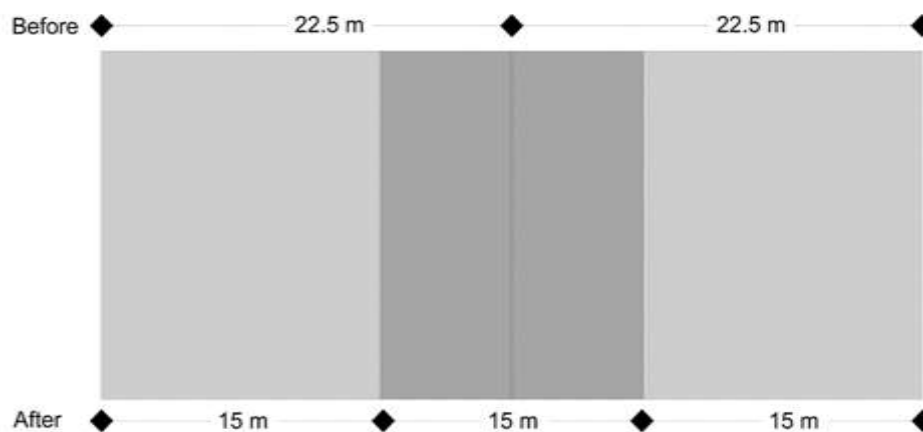
These requirements must be met for any proposed infill development. Consult the City of Swift Current's Zoning Bylaw, for exact requirements.

### 4.1 LOT WIDTHS

1. Single Lots – Infill development is subject to the City of Swift Current's Zoning Bylaw
2. Large Scale Site Infill Development – The large scale sites shall be subject to the City of Swift Current's Zoning Bylaw.

### 4.2 LOT CONSOLIDATION AND SUBDIVISION

Two or more lots may be consolidated, or consolidated and re-subdivided, provided the new lots meet the lot dimension requirements in the City of Swift Current's Zoning Bylaw.



**Figure 4-1**  
**Lot Consolidation and Subdivision**

## 5 Servicing Connections

These requirements must be met for any proposed infill development. Consult the City of Swift Current's Development Plan and related policy or guidelines for servicing requirements.

## 5.1 SERVICING CONNECTIONS

Any new development will require complete replacement of servicing connections from the development to the main line, unless otherwise approved by the City of Swift Current's Infrastructure and Operations Department.

Avoid splitting a single service line for more than one lot. Using one existing service connection to the main line for two adjoining units on separate lots is unacceptable. Any new services must be in accordance with City of Swift Current's Infrastructure and Operations design standards current at the time of the application submission.

## 5.2 LOT GRADING AND DRAINAGE

If a new lot configuration is created, a lot grading and drainage plan may be required for each lot.

# 6 Site Layout Requirements

These requirements provide for additional flexibility for proposed infill development. In general, consult the City of Swift Current's Zoning Bylaw for exact requirements.

## 6.1 FRONT YARD SETBACKS

The front yard setback for a new development will be the average of the front yard setbacks of the two adjoining properties. If there is a discrepancy of greater than 1.5 m in the setbacks of the building(s) on the two adjoining lots, the Development Officer has the discretion to consider the setbacks of other houses along both sides of the street when determining the required setback.

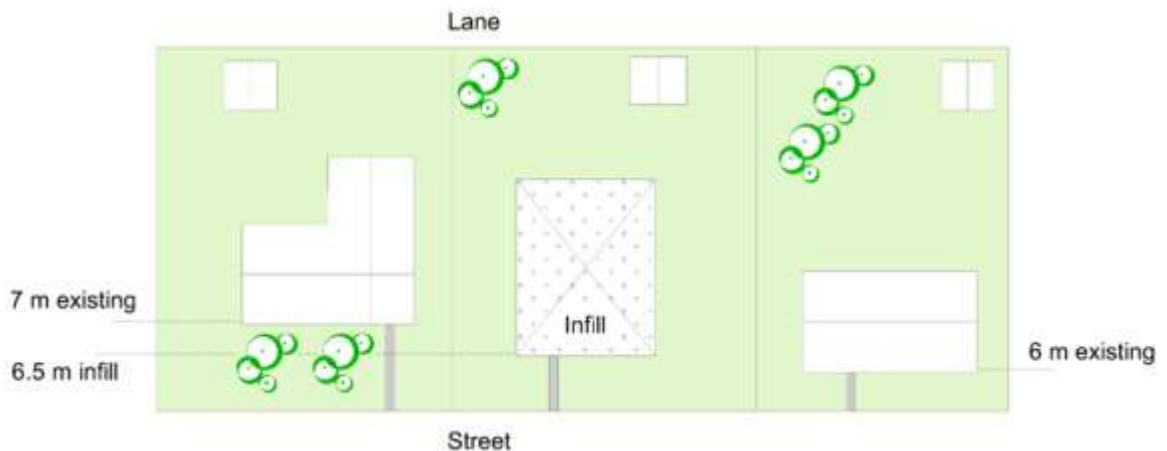


Figure 6-1  
Front Yard Setbacks

## 6.2 CORNER LOTS – FLANKING SIDE SETBACKS

For perpendicular lots on corners, the flanking side setback must be considered relevant to the front yard setback of the adjoining lot. Any development within the rear 40 percent of a perpendicular corner lot will have a setback at least 1 m greater than the flanking side setback of the remainder of the building.

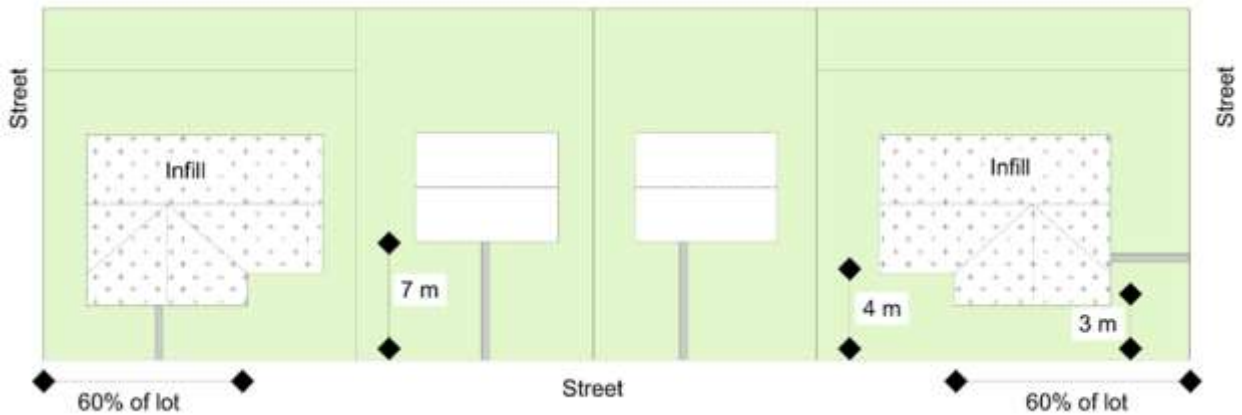


Figure 6-2  
Corner Lots – Flanking Side Setbacks

## 6.3 LOTS ADJACENT TO REAR LANES

If a lot backs onto an accessible rear lane, the lane should be used as the primary access to parking. Garages should not be located in the front yard when a lane exists. The Development Officer has the discretion to change these requirements if the lot's configuration, location or topography does not allow for such access.

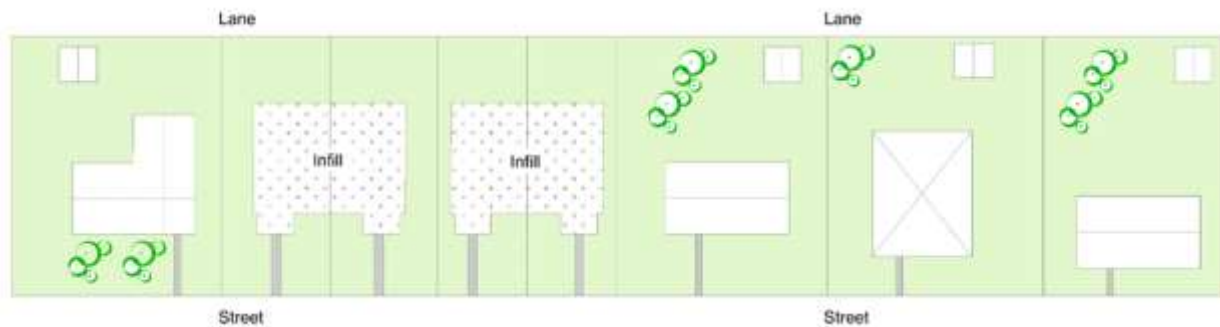
## 6.4 LOTS REQUIRING FRONT ACCESS

If lanes do not exist, the infill development should locate garages in the side yard or rear yard with access being provided to these garages from a driveway from the street. Garages should not be located forward of the residential area of the home.

One garage is permitted per single-detached, semi-detached or dwelling group unit. Access on corner lots must minimize the impact on intersections, and be in accordance with City of Swift Current's Infrastructure and Operations design standards and construction specifications current at the time of the application submission.

## 6.5 MULTIPLE LOT DEVELOPMENT

A multiple lot development includes four or more adjoining single-detached, semi-detached, or dwelling group buildings (minimum of four units) being developed at the same time. Where possible multiple family units should be designed to look like and operate as single family units with separate access locations and varied roof lines. Monolithic structures with little or no architectural articulation or variation in height are not encouraged as infill products in areas where single family duplex homes exist.



**Figure 6-3**  
**Multiple Lot Development**

If a multiple lot development is within a regular block, these regulations will be applied as if a single house were being built. The existing houses on either side of the proposed development will be used as guidelines for determining height, coverage, access, setbacks and depth with the every attempt made to blend the development design with adjacent houses.

## 7 Building Mass Requirements

These requirements must be met for any proposed infill development. Consult the City of Swift Current's Zoning Bylaw for exact requirements.

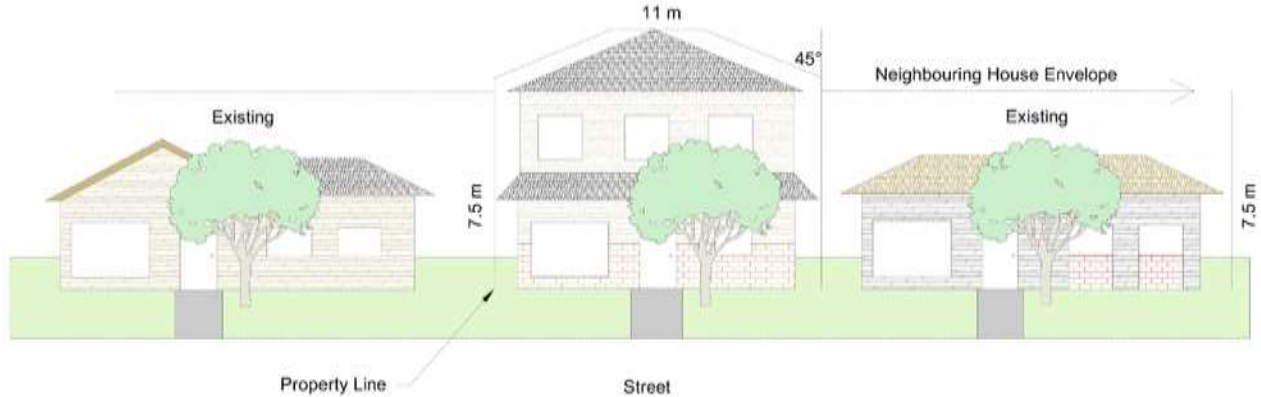
### 7.1 BUILDING HEIGHT

Where the established street is characterized by low profile bungalows, the height of the new infill or an addition must be particularly sensitive to building height. Greater design restrictions on initial infill developments may be required in order to ensure a more sensitive integration with the existing character and scale of the mature area.

When at least one of the adjoining houses on the streetscape, or both frontages for a corner lot, is less than 7.5 m in height, the new development cannot exceed more than 10% the height of the house that is adjoining with less than 7.5 m in height.

The restricted building envelope is formed by planes extended up 7.5 m perpendicularly from the property line. It then extends inwards and upwards at a 45° angle to a maximum height of 11.0 m.

There may be circumstances when a lower profile development is warranted. This would be determined at the discretion of the Development Officer based on site-specific considerations such as topography or shadowing.



**Figure 7-1  
Building Height**

The envelope does not apply where adjoining development is two storeys or greater in height.

For large scale infill development sites where multiple lots are proposed, the zoning regulations subject to the City of Swift Current's Zoning Bylaw shall apply.

## **7.2 LOT COVERAGE**

Coverage of a lot must be comparable to the existing adjoining lots. Lot coverage must be within 10 percent of the existing coverage for the low density development on the immediately adjoining lot which has the greatest lot coverage.

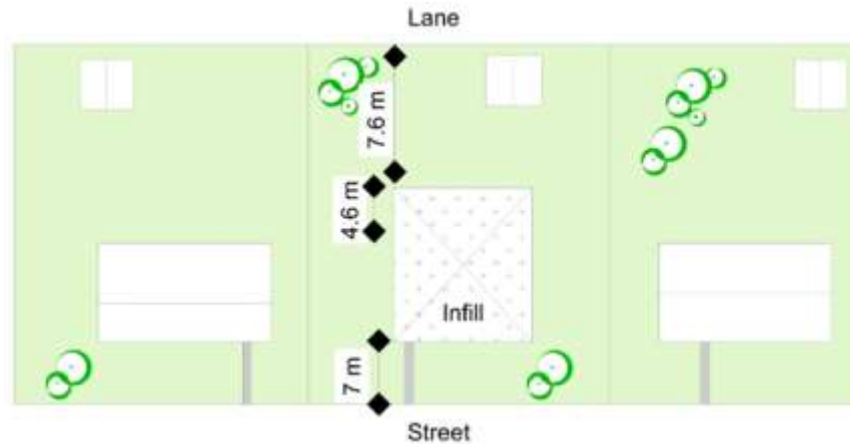
For large scale infill development sites where multiple lots are proposed, the zoning regulations subject to the City of Swift Current's Zoning Bylaw shall apply.

## **7.3 BUILDING LOT DEPTH**

The depth of the new infill lot, plus the required front yard setback, should not exceed the City of Swift Current's Zoning Bylaw regulations for yard requirements. The remaining rear yard of the lot is to be used for outdoor amenity space and accessory buildings. In addition, the actual house should not extend more than 4.6 m beyond the rear of the adjoining houses.



For large scale infill development sites where multiple lots are proposed the zoning regulations subject to the City of Swift Current's Zoning Bylaw shall apply.



**Figure 7-2**  
**Building Lot Depth**

## 8 Landscaping

These requirements must be met for any proposed infill development. Consult the City of Swift Current's Zoning Bylaw for exact requirements.

### 8.1 RETAIN HEALTHY MATURE TREES

Infill developments should enhance the streetscape through the retention of healthy mature trees and planting. Every attempt should be made to preserve existing mature trees in front yards, or front and side yards for corner lots, by designing around them if they do not impede development.

Any mature trees which cannot be retained should be replaced by other landscaping, at the discretion of the Parks Manager and/or Development Officer.

If there are no existing mature trees on the site that can be preserved, trees must meet the standards outlined the City of Swift Current's Zoning Bylaw.

### 8.2 LANDSCAPING

A landscaping plan shall be submitted, except in the case of single-detached, semi-detached and two-unit dwellings, indicating all physical features including both existing and proposed grades, planting and construction material and shall meet the standards outlined in the City of Swift Current's Zoning Bylaw.

## 9 Design and Façade Guidelines

In addition to the requirements, it is recommended that the following design guidelines be considered for any proposed infill development. The intent of these guidelines is to ensure a visual cohesiveness for streetscapes in established mature areas as new houses are built. The guidelines are intended to be used by developers to become aware of, and to respect, the existing mature area of the City's character when bringing forward a proposed infill development. It is intended that existing incompatible development in a mature area of the City is not to be used as a reference. Where it is warranted, these performance expectations and guidelines can result in the creation of an architectural control district overlay or a direct control district.

### 9.1 ROOF TYPE

Roof style is one of the defining elements of a house. It is important that new houses have a roof style that reflects that of adjoining houses.

For instance, many of the houses in the established mature areas of the City have low pitch side gable or cross gabled roofs giving a fairly strong horizontal line to the streetscape. A hipped roof would be more compatible with these than a high pitched front gable roof.

Design elements on the new house façade can also be used to reflect the roof styles and pitches of neighbouring houses and maintain a continuous flow to the streetscape, allowing for more variety in roof styles. This is especially beneficial when a new house is larger than the existing ones.

### 9.2 BREAK UP LARGER FACADES

The building façade of a larger building could be broken into smaller elements to create the illusion of a smaller structure and better fit with smaller houses along the street.

Large plain outer wall surfaces on front walls, or flanking front walls, should be avoided. Windows or design elements should be used to break up such a surface.

Avoid disrupting the existing rhythm of building width and spacing along a block with inappropriate side setbacks.

A new, narrower building set significantly closer to one side of the lot on a street where the buildings are centred on the lot should be avoided.

### 9.3 MATERIAL, TEXTURE AND COLOUR

The selection of exterior materials and texture for new construction should be compatible with the surrounding buildings and the existing adjoining buildings. In areas where strong continuity of materials and textures is a factor, the continued use of those materials should be incorporated into the construction effort.

Materials that are uncommon in the area, such as stonework or brick, should be considered for accent purposes only.

Dominant house colours in established mature areas of the City tend to be muted. Vibrant colour should be used with discretion and in small amounts.

#### **9.4 ENTRIES AND FRONT WINDOWS**

The porch and entry of a house help determine the character of a structure. The rhythm established by the continuous patterns of entries along a street intensifies its streetscape. Front entries should be clearly identifiable from, and face onto, the street.

The height of the entry should reflect that of neighbouring houses.

Window styles should be of similar proportion, size, shape and detailing as neighbouring houses. They should also be of a similar number, composition and placement. For instance, if gliding windows are predominant in the neighbouring houses, then casement or gliding windows would be preferable in the new house to double-hung windows.

#### **9.5 WINDOWS, DOORS, DECKS AND BALCONIES**

Windows and above-ground decks should be placed and oriented to face away from neighbouring yards. Side windows should be placed to avoid looking directly into a window in the facing house. Avoid placing windows that look directly into a neighbour's window or overlook the neighbouring rear yard. Glazing or opaque windows should be considered on the side to allow light to the new house, but respect the privacy of the existing house and yard.

Avoid second storey balconies, especially open balconies, if there are over-viewing problems. A recessed second storey balcony would be preferable.

Topography should be taken into consideration when assessing privacy and overlooking issues.

#### **9.6 CORNER LOTS - FAÇADE DETAILS**

On corner lots, both building elevations facing the street must be given equal treatment. Elements such as corner and side windows, wrap-around verandas or decks, or other interesting architectural elements should be incorporated into the design.

Each building elevation must be compatible with other houses on the respective adjoining streetscapes.

For the flanking frontage, graduated setbacks for different portions of the house may be required; this should be balanced with setbacks for adjoining houses.

The design of a garage must consider adjoining house facades if the garage is located on one of the frontages. The number of vehicles stored on site should be in scale with the character of the area.

Semi-detached infill developments on corner lots should make every effort to have the front entries on separate frontages of the lot.

Where rear yard corner development is feasible, every effort must be made to design the project so that it is sensitive to the adjoining properties including, but not limited to, building orientation, building setbacks, landscape buffering, privacy, screening, etc. The Development Officer may allow for flexibility in a site plan that can be mutually agreed upon.

## **10 LARGE SITE INFILL DEVELOPMENT**

Significant increases in density and the introduction of large scale infill development into existing mature areas generates a number of challenges which vary from site to site. In turn, the planning and design solutions are dependent upon the specific site and the area context in which it is located. Thus, the challenge is to derive general planning and design guidelines that give guidance to resolving the challenges and provide the flexibility to deal with the site context.

The primary challenges that must be addressed in planning and designing a large scale site in the context of existing, mature areas of the City include:

### **10.1 INTEGRATION WITH EXISTING NEIGHBOURHOODS**

Integrating the new development into the fabric and street patterns of adjacent mature areas should be the main objective of the infill development.

1. Surrounding street patterns (roads and lanes) should be extended in and through any large site to break down the scale of the redevelopment site to planning units which reflect the typical City block of 1.5 hectares and to enhance connectivity between adjacent mature areas.
2. The street network should divide large sites into smaller, connected blocks and form the basis of vehicular and pedestrian movement, including connections to open space and area facilities.
3. Buildings should be sited and designed to have their major entries and as many individual dwelling unit entries as possible fronting onto the adjacent and internal streets, major pathways and major open spaces.

## 10.2 LIVABLE NEIGHBOURHOOD

Planning a livable new area should provide for a high quality, well designed and livable area that contributes to Swift Current as a community and is economically sustainable.

1. Buildings should be organized to optimize sunlight to other buildings on the site and in the adjacent mature area, and to public open spaces including streets.
2. Block arrangement and site design should respond to natural features, public open space and the existing mature area traffic patterns.
3. Buildings should be organized to ensure adequate spacing to maximize livability, ensure privacy and provide views through the site.

## 10.3 TRANSPORTATION AND PEDESTRIAN SYSTEM

Building a complete transportation and pedestrian system should minimize the impact of traffic and parking on adjacent mature areas of the City. It also provides for the creation of a pedestrian oriented area that contributes to environmentally sustainable transportation opportunities.

1. The pedestrian/cyclist network should be an integral aspect of site design, and provide for:
  - a. Internal connections for residents to area facilities and amenities;
  - b. External connections to facilities and amenities outside the infill site, including safe routes to school for children;
  - c. Public rights of way to enhance pedestrian/cyclist circulation through the site, with particular attention to connecting parks and other public amenity sites;
  - d. Connections to the adjacent residential area, including access to public facilities on the site and routes through the site; and
  - e. The security of common areas within individual development parcels.
2. Where more than one large infill site is being developed in an area, or a second site is likely to develop in the near future, traffic studies should consider the cumulative impact of all potential sites on an existing mature area.
3. Off-street parking should be planned and provided as an integral part of large infill site development and in accordance with the parking regulations in the City of Swift Current's Zoning Bylaw.

### 10.4 PARKS AND AMENITY SPACE

Enhancement and retention of existing parks and amenity space should be incorporated to ensure that the recreation and open space needs of new and existing residents are met.

1. The location and shaping of public open space should be fundamental in organizing the block structure and locating development on large sites.
2. Onsite parks, open space and community amenities should be provided which:
  - a. Are sufficient to meet the needs of new residents;
  - b. Are appropriate for families with children;
  - c. Are integrated with and will complement existing community parks and amenities in the adjacent mature areas of the City;
  - d. Are available for use by the adjacent community; and
  - e. Are designed for a range of functions and seasons.
3. The specific amount of open space to be provided should be determined based on the size and population of the proposed development and existing open space located in proximity to the infill site.
4. Parks and community amenities should be designed and constructed to a high standard and should be:
  - a. Integrated into the overall site design;
  - b. Located so that the space can be monitored by the residents; and
  - c. Protected from negative impacts such as shadowing, traffic and noise.

### 10.5 BUILDING COMMUNITY

Building community should be considered by building a socially sustainable community.

1. Site design should reflect the need to accommodate families with children, including:
  - a. Safe pedestrian routes to schools;
  - b. Common parks/activity areas and open space suitable for and readily accessible by children;
  - c. Outdoor play areas that reflect the needs of different age groups of children; and
  - d. Accommodating supervision and oversight of play areas by parents/caregivers.
2. Semi-private and private outdoor spaces and indoor and outdoor amenities should be provided to meet the recreational and social needs of residents, including families with children.
3. Residential infill projects on large sites should include a variety of housing types to provide housing choices for households of different sizes, types (family, non-family), ages and incomes.
4. Non-market housing should be provided in accordance with the existing City Housing Plan for the provision of affordable housing.

## 10.6 LANDSCAPE AND STREET DESIGN

Landscape and street design should create comfortable pedestrian environments and attractive interface areas with adjoining mature areas.

1. A landscape plan prepared by a professional landscape architect, and providing for a high standard of landscaping, should accompany any development proposal for a large infill site.
2. The landscape plan should include, as a minimum:
  - a. A design rationale;
  - b. A public open space plan;
  - c. A streetscape design, including tree planting, boulevard landscaping, street furniture, sidewalk treatment, location and treatment of transit stops;
  - d. Highlights of retained or altered natural landscape conditions;
  - e. Specific treatment of surfaces and plantings;
  - f. The type and quantity of landscape materials, including the use of drought tolerant plants and native species;
  - g. The treatment of streets fronting onto adjacent, existing residential areas; and
  - h. Documentation of how water consumption has been minimized through features such as, but not limited to, the use of high efficiency irrigation systems and xeriscaping.
3. Street enhancements (such as double rows of street trees, boulevards, sidewalks, street furniture) should be designed to a high quality with a focus on the pedestrian and should be provided on new streets and on existing streets impacted by the development.
4. The site design and site layout should reflect existing site conditions and incorporate:
  - a. Existing trees on the site; and
  - b. Natural water courses or any other significant natural feature, including the retention of these features where possible.

## 10.7 PHASING OF DEVELOPMENT

Phasing of development should sequence the construction of large infill projects to reduce the impact on adjacent mature areas.

1. In order to reduce the impact of infill construction and to ensure a timely transition between existing and new development, phased development projects should:
  - a. Develop the edge of the site where it fronts existing residential use in the first phase of the project in order to prevent empty or underutilized lots facing the surrounding mature areas; and,
  - b. Renovate or refurbish existing buildings that are retained in the first phase.

## 11 PLANNING AND PUBLIC CONSULTATION

### 11.1 SINGLE LOT

Infill development is subject to the City of Swift Current's Zoning Bylaw. If a rezoning is required the requirements for public consultation for rezoning would apply.

### 11.2 LARGE SCALE SITE

Large scale sites are areas which contain a minimum of 1.5 hectares of land, a former school site regardless of size, or any other site identified in the City of Swift Current's Housing Plan.

### 11.3 CONCEPT PLAN

The Developer shall prepare concept plans for large scale sites. These concept plans will bring certainty for both the surrounding residents and potential developers. A concept plan provides a detailed layout of an anticipated development for an area based on expected land uses, road design, location of services, drainage needs, and density or type of development. Concept plans must be in conformity with the City of Swift Current's Development Plan, and the Infill Guidelines provided. The concept plan is adopted by bylaw and becomes part of the City of Swift Current's Development Plan. Concept plans must be developed in conformity with the following process:

1. Concept Plan – The concept plan shall address the types of housing needed and the associated amenities required for the area. The concept plan should be completed in conformity with the infill guidelines and display the land uses that are proposed for the site.
2. Public Consultation – Upon completion of a proposed draft concept plan, the City Clerk shall give notice of the time and place at which the City will host an Open House and shall publicly hear and receive any written comments or concerns to the proposed draft concept plan. Such notice shall be by advertisement inserted at least once a week for two (2) successive weeks in a newspaper published within the City and by serving a copy of the said notice on all of the assessed property owners within a radius of 75.0 metres of the proposed draft concept plan.
3. Finalization of the Concept Plan – Upon completion of the Open House, the City of Swift Current shall ensure concerns raised by the attendees are considered and, if feasible, addressed in the finalized concept plan.
4. Adoption by Council – After completion of the concept plan, the City Council will consider adoption of the concept plan and if adopted will be by a bylaw appending the concept plan to the City's Development Plan.

### 11.4 DEVELOPMENT APPLICATION

Developers shall be required to complete a development permit application for a large scale site development plan. Development permit applications must be in conformity with the City of Swift Current's



Development Plan, the Infill Guidelines, and the adopted concept plan for the area. The development application shall be in conformity with the following process:

1. Development Application – The developer shall prepare a development application with a development plan that is in conformity with the adopted concept plan and submit it to the City's Planning and Growth Department.
2. Initial Review – The City administration reviews the plan to ensure conformity with the concept plan and conformity with other regulations of the City.
3. Public Consultation – Upon review of the proposed development application, the City Clerk shall arrange and give notice of the time and place at which the developer will host a public meeting to present the development plan. This shall provide the public an opportunity to hear and provide any written comments or concerns to the proposed development plan. Such notice shall be by advertisement inserted at least once a week for two (2) successive weeks in a newspaper published within the City and by serving a copy of the said notice on all of the assessed property owners within a radius of 75.0 metres of the proposed development plan. This is the developer's public meeting and the developer shall pay a fee to the City to recoup the cost of the public meeting mail-outs, advertisements, refreshments, and any other disbursements.  
The City's planning staff shall be in attendance at the Developer's public meeting to record comments and concerns from the attendees. The staff shall answer questions from the attendees regarding the City's regulations and infill guidelines.
4. Finalization of Application – The Developer finalizes the development application following feedback received from the public meeting and the City's record of comments and concerns.
5. Final Review – The City reviews the final development plan application in detail with recommendations on appropriate zoning for the site and submits a report to Council for its decision. If Council concurs with the development plan, the Council shall direct staff to prepare the bylaws for appropriate zoning changes. The zoning process then begins according to the requirements of *The Planning and Development Act, 2007*.
6. Direct Control District – Consideration should be given to utilize a Direct Control District for the implementation of a large scale site development plan. In this way the approved development plan application with all site dimensions, type of housing form, heights, landscaping and other details approved in the plan becomes the Direct Control District and development must adhere to the plan. Any changes would require an amendment to the Direct Control District. This will bring certainty to the future development and ensure that what was approved is built in the same manner as specified in the approved development plan.



# REPORT

## Closure

In conclusion, the City of Swift Current Infill Guidelines Report reflects the needs and desires of the City for a new development that is sited on vacant or undeveloped land within the existing community.

The services provided by Associated Engineering (Sask.) Ltd. in the preparation of this report were conducted in a manner consistent with the level of skill ordinarily exercised by members of the profession currently practicing under similar conditions. No other warranty expressed or implied is made.

Respectfully submitted,  
Associated Engineering (Sask.) Ltd.

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